



Stephen R. Reed, Mayor • Harrisburg City Council



City of Harrisburg Subdivision and/or Land Development Application

Project Name or Plan Title

Primary Property Address

Tax Parcel ID Number

Please List All Property Addresses And Tax Parcel ID Numbers Involved In The Project (*Use Additional Sheets If Necessary*)

Zoning District(s)

Please explain the purpose of the project. Provide as much information and be as specific as possible. (*Use Additional Sheets If Necessary*)

Application Type (check one):

- ☐ Preliminary Subdivision
- ☐ Final Subdivision
- ☐ Combined P/F Subdivision
- ☐ Preliminary Land Development
- ☐ Final Land Development
- ☐ Combined P/F Land Development
- ☐ Combined P/F Subdivision & LDP
- ☐ Lot Add-On Plan
- ☐ Planned Residential Development

Please provide the following information about this project:

1.) Square footage of non-residential buildings	sq. ft.
2.) Number of resulting lots	lots
3.) Number of buildings / units to be built	_____ # of units _____ # of bldgs
4.) Is a new street proposed? (yes/no)	<input type="checkbox"/> Yes <input type="checkbox"/> No
5.) What is the proposed name for the new street?	
6.) How many lineal feet will the proposed street be?	feet

What Waivers Are Being Requested?

Use the attached checklist to determine the waivers you are requesting.

Applicant

Name _____

Company _____

Address _____

Phone _____

Email _____

Applicant's Status

(Circle One)

Owner

Lessee

Equitable Owner

Contract Purchaser

Main Contact for the Project

Name _____

Company _____

Address _____

Phone _____

Email _____

Site Plan Designer

Name _____

Company _____

Address _____

Phone _____

Email _____

Property Owner

Name _____

Company _____

Address _____

Phone _____

Email _____

Items To Be Submitted With This Application.

Failure to do so WILL delay the review process.

- | | |
|--|--|
| <input type="checkbox"/> Check made payable to the “ City Treasurer ” for the correct amount (see the fee schedule) | <input type="checkbox"/> Elevation drawings including all visible sides and façade materials |
| <input type="checkbox"/> Dauphin County Planning Commission application and fee (check made payable to “ Dauphin County Planning Commission ”) | <input type="checkbox"/> Landscaping Plan |
| <input type="checkbox"/> Twenty-one (21) copies of the plans:
Four (4) sets of 24” x 36”
Seventeen (17) sets of 11” x 17” or half size | <input type="checkbox"/> Lighting Plan |
| <input type="checkbox"/> Following the Planning Commission Meeting –
Three (3) sets of 24” x 36” will be provided with any corrections requested by the HPC or City Staff | <input type="checkbox"/> Stormwater management plans |
| <input type="checkbox"/> Completed Checklist (use attached document) | <input type="checkbox"/> Parking and circulation plan |
| | <input type="checkbox"/> Traffic Study (if required by the City Engineer) |
| | <input type="checkbox"/> A D.E.P. Sewer Module or Waiver |

If already submitted, please note the submittal date:

APPLICANT / OWNER CERTIFICATION

I hereby certify that the proposed work is authorized by the owner of record and that I agree to conform to all applicable laws of this jurisdiction. I understand that any falsification could lead to denial or criminal penalties, or revocation of any permit pursuant to this application. I agree that work will not commence prior to final approval.

_____ Applicant's Signature	_____ Date	_____ Property Owner's Signature	_____ Date
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The owner must sign this application. The applicant signature is required when different from owner.

HARRISBURG LAND USE BOARDS / 2004 SCHEDULE

(All meetings take place in Room 213, City Government Center, unless otherwise noted.)

Harrisburg Architectural Review Board

Meeting time: **6:00 P.M.**

Meeting Date (1 st Monday unless noted)	Application Submission Deadline (Friday)
JANUARY 5	<i>December 19, 2003</i>
FEBRUARY 2	<i>January 23</i>
MARCH 8 (2 ND MONDAY)	<i>February 20</i>
APRIL 12 (2 ND MONDAY)	<i>March 26</i>
MAY 3	<i>April 23</i>
JUNE 7	<i>May 21</i>
JULY 12 (2 ND MONDAY)	<i>June 25</i>
AUGUST 2	<i>July 23</i>
SEPTEMBER 13 (2 nd Monday)	<i>August 27</i>
OCTOBER 4	<i>September 24</i>
NOVEMBER 8 (2 ND MONDAY)	<i>October 29</i>
DECEMBER 6	<i>November 24 (Wednesday)</i>
Deadline for January 3, 2005 meeting: <i>December 17, 2004</i>	

Harrisburg Planning Commission

Meeting time: **6:30 P.M.**

Meeting Date (1 st Wednesday unless noted)	Application Submission Deadline (Friday)
JANUARY 7	<i>December 12, 2003</i>
FEBRUARY 4	<i>January 9</i>
MARCH 3	<i>February 6</i>
APRIL 14 (2 ND WEDNESDAY)	<i>March 12</i>
MAY 5	<i>April 8 (Thursday)</i>
JUNE 2	<i>May 14</i>
JULY 7	<i>June 11</i>
AUGUST 4	<i>July 9</i>
SEPTEMBER 1	<i>August 13</i>
OCTOBER 13 (2 ND WEDNESDAY)	<i>September 10</i>
NOVEMBER 3	<i>October 8</i>
DECEMBER 1	November 12
Deadline for January 5, 2005 meeting: <i>December 10, 2004</i>	

Zoning Hearing Board

Meeting time: **6:00 P.M.**

Meeting Date (2 nd Monday unless noted)	Application Submission Deadline (Friday)
JANUARY 12	<i>December 12, 2003</i>
FEBRUARY 9	<i>January 9</i>
MARCH 15 (3 RD Monday)	<i>February 6</i>
APRIL 19 (3 RD Monday)	<i>March 12</i>
MAY 10	<i>April 8 (Thursday)</i>
JUNE 14	<i>May 14</i>
JULY 19 (3 RD MONDAY)	<i>June 11</i>
AUGUST 9	<i>July 9</i>
SEPTEMBER 20 (3 rd Monday)	<i>August 13</i>
OCTOBER 18 (3 rd Monday)	<i>September 10</i>
NOVEMBER 15 (3 rd Monday)	<i>October 8</i>
DECEMBER 13	<i>November 12</i>
Deadline for January 10, 2005 meeting: <i>December 10, 2004</i>	

Subdivision and Land Development Applications Fee Schedule

	Subdivision Fees	Land Development Fees
Combined Preliminary/ Final Plan	\$175 + 25 per lot	\$175 + 25 per lot or 5,000 square feet of floor area
Preliminary Plan	Same as above	Same as above
Final Plan	\$150 + 25 per lot	\$150 + 25 per lot or 5,000 square feet of floor area
Lot Add-on	\$125	\$125
Revised Final Plan	\$100	\$100
Waiver	\$25 per waiver	\$25 per waiver

*Should a public hearing be required, the applicant shall reimburse the City for the cost incurred for publication of legal notices, for any associated mailing costs, and for any stenographic services utilized for the hearing.

Dauphin County Planning Commission

Application for Plan Review / Approval

Municipality _____ Plat Title _____

Surveyor _____ Engineer _____

Plan Classification: ☐ Subdivision ☐ Preliminary
☐ Land Development ☐ Final
☐ Combined ☐ P/F
☐ Minor

Owner/Developer _____ Phone Number _____

Address _____

Total Tract Area: _____ Subdivided Area: _____ (Subdivision)

Developed Area: _____ (Land Develop.)

Existing # of Lots: _____ Proposed # of Lots: _____ Proposed # of New Dwelling Units: _____

Existing Sewerage System ☐ Public ☐ On-Lot ☐ None Proposed Sewerage System ☐ Public ☐ On-Lot ☐ None

Existing Water System ☐ Public ☐ Well ☐ None Proposed Water System ☐ Public ☐ On-Lot ☐ None

Zoning District: _____

Existing Land Use: _____ Proposed Land Use: _____

Are any zoning variances/subdivision waivers requested? ☐ Yes ☐ No

List variances/waivers requested: _____

Date plan to be reviewed by Municipal Planning Commission: _____

Purpose of the Plan: _____

Fees Submitted: \$ _____ Check Number _____ Signed by: _____ Date: _____

****The Dauphin County Planning Commission meeting is held on the first Monday of each month. The cut off date for plan submission is nine (9) days prior to the meeting.*

Please submit this application and one (1) copy of the plan for review, or six (6) copies of the plan for approval to:

Dauphin County Planning Commission
Veterans Memorial Office Building
112 Market Street, 8th Floor
Harrisburg, PA 17101
(717) 234-2639

DAUPHIN COUNTY SUBDIVISION AND LAND DEVELOPMENT PLAT FEE SCHEDULE

REVIEWS

The fee schedule for Subdivision and Land Development Plats submitted for Dauphin County Planning Commission Review and Comment action is as follows:

1. Separate Subdivision and Land Development Applications

Subdivision by Lots

\$70.00 base fee, plus
\$2.00 per lot for first 25 lots, plus
\$1.00 per lot over 25 lots.

Residential Land Development

\$70.00 base fee, plus
\$2.00 per dwelling unit, for first 25 dwelling units, plus
\$1.00 per dwelling unit over 25 dwelling units.

Non-Residential Land Development

\$70.00 base fee, plus
\$2.00 per acre (or fraction thereof), first 10 acres, plus
\$1.00 per acre (or fraction thereof), over 10 acres.

2. Combined Subdivision and Land Development Applications

Residential

\$70.00 base fee, plus
\$2.00 per lot, first 25 lots, plus
\$1.00 per lot over 25 lots, plus
\$2.00 per dwelling unit, first 25 dwelling units, plus
\$1.00 per dwelling unit over 25 dwelling units.

Non-Residential

\$70.00 base fee, plus
\$2.00 per lot, first 25 lots, plus
\$1.00 per lot over 25 lots, plus
\$2.00 per acre (or fraction thereof) first 10 acres, plus
\$1.00 per acre (or fraction thereof) over 10 acres.

All fees are to be paid by check, made payable to the "Dauphin County Planning Commission."

Effective Date: 11/7/94 Revised: 9/22/99

CHAPTER 7-511

IMPROVEMENT AND CONSTRUCTION REQUIREMENTS

Section	7-511.1	Improvements Required
Section	7-511.2	Monuments and Markers
Section	7-511.3	Streets and Driveways
Section	7-511.4	Sewers and Water
Section	7-511.5	Surface Water Management
Section	7-511.6	Curbs
Section	7-511.7	Sidewalks
Section	7-511.8	Fire Hydrants
Section	7-511.9	Shade Trees
Section	7-511.10	Street Signs

CROSS REFERENCES

7-511.1 IMPROVEMENTS REQUIRED

The applicant shall provide all improvements required by these regulations pursuant to the specifications for the improvements contained herein or in the City Engineering Specifications or in the Utilities and Public Services Code. (Ord. 23-1990.)

7 -511.2 MONUMENTS AND MARKERS

(a) Monuments must be set:

- (A) at the intersections of all street right-of-way lines;
- (B) at the intersection of lines forming angles in the boundaries of the Subdivision or Land Development;
- (C) or at such intermediate points as may be required by the City Engineer.

(b) Markers must be set:

- (A) at all corners except those monumented;
- (B) by the time the property is offered for sale.

(c) Monuments and markers shall be the following sizes and made of the following materials:

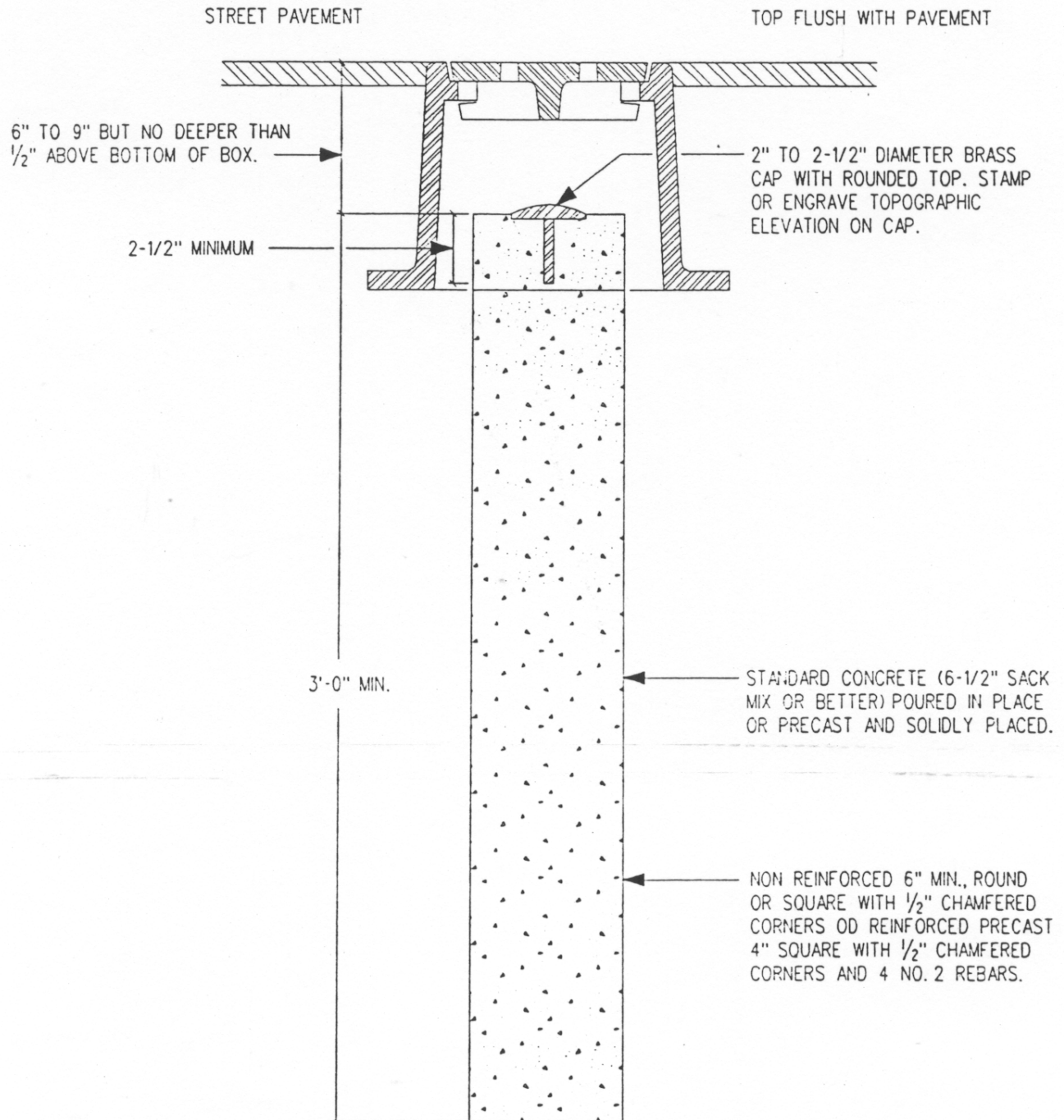
- (1) Monuments shall be six (6) inches square or four (4) i nches in diameter and shall be thirty-six (36) inches long. Monuments shall be made of concrete, stone, or by setting a four (4) inch cast or steel pipe filled with concrete.
- (2) Markers shall be three-quarters (3/4) of an inch in diameter and thirty-six (36) inches long. Markers shall be made of iron or steel bars.

- (d) Monuments and markers must be placed so that the scored or marked point coincides exactly with the point of intersection of the lines being monumented. They must be set so that the top of the monument or marker is level with the finished grade of the surrounding ground. Monuments must be marked on top with a copper or brass plate or dowel set in the concrete. (Ord. 23-1990.)

7-511.3 STREETS AND DRIVEWAYS

- (a) Streets must be surfaced to the grades and dimensions drawn on the plans, profiles, and cross-sections submitted by the applicant and approved by the City Engineer. Before paving the street surface, the applicant must install the required utilities and provide, where necessary, adequate storm water drainage for the street acceptable to the City Engineer. The pavement base and wearing surface must be constructed in accordance with Pennsylvania Department of Transportation Publication 70 and material specifications in Form 408.
- (b) Driveway entrances or aprons within the street right-of-way shall be surfaced to their full width. The width of access roads or driveways shall be in accordance with the following standards:
 - (1) For multi-family residential, mobile home parks, and all Non-Residential Developments or subdivisions, access roads shall be no less than twenty-four (24) feet in width at the street line, and shall clearly be defined by the use of curbing.
 - (2) For Single Family Residential Subdivisions, driveways shall be no less than ten (10) feet in width and no greater than twenty (20) feet in width at the street line.
- (c) Where proposed driveways provide access onto a State Highway, the design of such driveway access and drainage shall be prepared in accordance with the requirements of the Pennsylvania Department of Transportation and shall be subject to the approval and issuance of permits by that Department.)
- (d) Where a driveway or access road gives access to a municipal street, approval by the City Engineer shall be required and a Highway Occupancy Permit obtained.

NEENAH FOUNDRY CO. (PATTERN NO. R-1970) (PATTERN NO. R-1971) OR APPROVED EQUAL.



THE CITY OF HARRISBURG
OFFICE OF THE CITY ENGINEER

PROJECT: STANDARD MONUMENT DETAIL

DRAWN BY: MICHAEL J. HILTON

DATE: 25 SEPTEMBER 1996

SHEET:

D-23



City of Harrisburg Subdivision and/or Land Development Checklist

Please use this checklist as a guide for submitting a Subdivision or Land Development Plan. The content of this checklist is drawn from Chapter 7-5 of the Subdivision and Land Development Code and will be used by the Planning Staff to evaluate your plans; however, it should not be used as a replacement for, or in lieu of the SALD Code. Applicants are encouraged to refer to the Code when preparing plans.

DRAFTING STANDARDS

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
1	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.3(d)	Twenty-one (21) copies of the plans shall be submitted with the application. Four (4) sets shall measure 24" by 36" (required by Dauphin County Recorder of Deeds), and seventeen (17) sets shall measure 11" by 17" or half sheet size
2	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.3(b)	Dimensions shall be in feet and decimals; bearings shall be in degrees, minutes and seconds. Lot line descriptions shall read in a clockwise direction

All plans should include a cover sheet following the guidelines listed below. The preferred layout of the plan is represented in Figure 1.

COVER SHEET

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
3	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(1)	Title of the project
4	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.1(A&B) 505.4(3) 505.10(1)	Name, address, phone number of the owner of the tract, the developer/ subdivider, and the firm that prepared the plans
5	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(4) 505.10(1)	File Number assigned by the firm, the plan date, and the date(s) of all revisions
6	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(7) 505.10(1)	The total acreage of the tract or project
7	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(8) 505.10(1)	The district, setbacks, lot size and/or density requirements of the City's Zoning Ordinance
8	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.1(c)	A location map, relating the project area to its location in the City
9	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(12) 505.10(8)(l)	A statement on the Plan indicating that all zoning approvals have been obtained
10	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(13) 505.10(8)(m)	A statement on the Plan indicating any existing or proposed waivers have been granted by City Council
11	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(8)(o)	A source of title to the land, as shown by book of the Dauphin County Recorder of Deeds (for Final Plans)
12	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(e) 505.10(9) (f)(i)	Certification of the engineer, surveyor or landscape architect, to the effect that the plans are accurate (See Appendix No. 1 in the SD/LD Code)
13	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(e) 505.10(9) (f)(iii)	Signature block as it will appear on the Final Plan (See Appendices No. 2 A to 2-C and No. 3 A to 3 B)
14	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(9) (f)(iv)	Notation space to accommodate the Dauphin County Recorder of Deeds information (including Plan Book, Volume and Page)

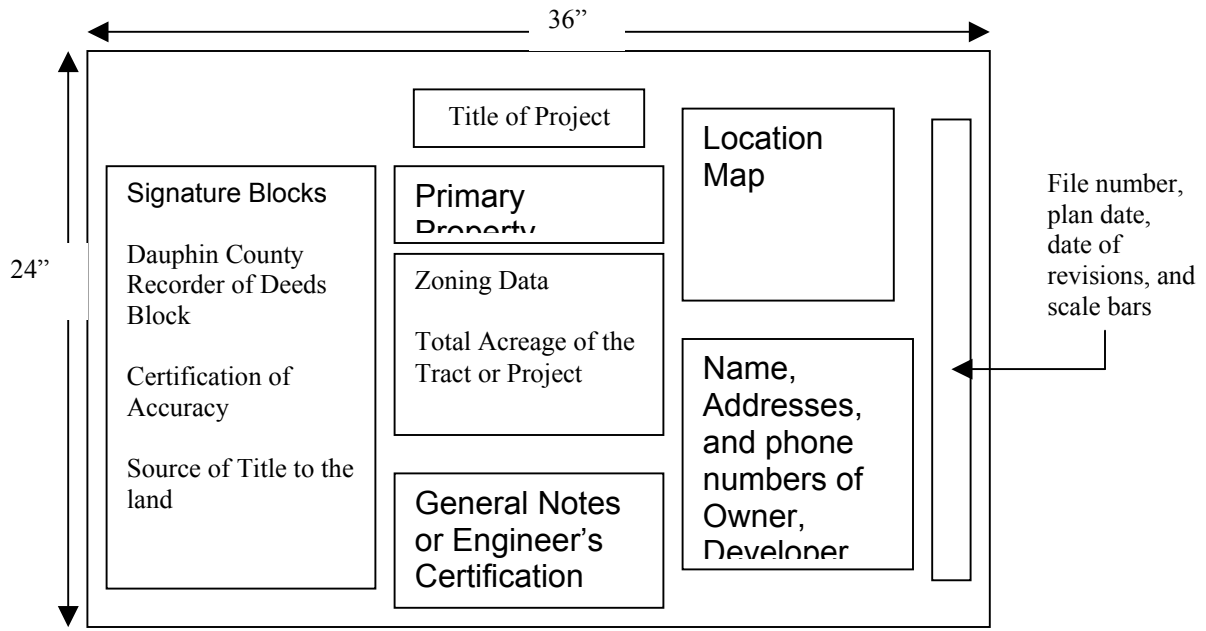


Figure 1

BASIC PLAN REQUIREMENTS

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
15	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.9(1) 505.10 (11)(A)	Check or money order made out to "City of Harrisburg Treasurer" in an amount specified on Subdivision/Land Development Fee Schedule
16	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.9(2) 505.10 (11)(B)	Check made out to Dauphin County Planning Commission in an amount specified on Subdivision/Land Development Fee Schedule
17	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(2)	Location of any municipal boundary line (if applicable)
18	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(5)	A north arrow, a graphic scale and a written scale
19	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(6)	The entire existing tract boundary with bearings and distances
20	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(7)	The total acreage of each tract involved with the project
21	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(9)	The location of existing lot line markers along the perimeter of the existing tract
22	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(10)	A location map, relating the project site to at least 2 intersections of street centerlines
23	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.4(11)	Property address(es) as assigned by the City's Registrar of Real Estate
24	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(1) 505.10(3)	Existing contours, at an interval of 2 feet for land with an average slope of four percent (4%) or less and at a minimum vertical interval of 5 feet for more steeply sloping land
25	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(2) 505.10(4)	Names of all adjacent landowners and the names and plan book numbers of all recorded plans for adjacent projects
26	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(5)	Location of historic structures, wetlands, unbuildable portions of the site, or other significant features
27	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(1) 505.10(8)(A)	Layout of streets, alleys, sidewalks, including cartway and rights-of-way widths. For Final Plans this shall include distances and bearings, with curve segments comprised of radius, tangent, arc and chord
28	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	7-505.6(2)	Lot lines, with accurate bearings and distances and lot areas for

				505.10(8)(B)	all parcels
29	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(3) 505.10(8)(C)	Block and lot numbers in consecutive order
30	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(5) 505.10(8)(E)	Total number of lots, units of occupancy, density, and proposed land use
31	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(6) 505.10(8)(F)	Easements, rights-of-way
32	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(7) 505.10(8)(G)	Building setback lines with distances from front, rear and side yard property lines
33	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(8) 505.10(8)(H)	Identification of buildings to be demolished
34	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(9) 505.10(8)(I)	Typical cross section of each street <i>proposed</i> or to be <i>improved</i> as part of the application. Cross section shall include entire right-of-way width
35	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(14) 505.10(8)(N)	Street Names
36	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(8)(P)	Location and material of all permanent monuments and lot line markers, noting that all monuments and markers are set, or indicating when they will be set (for Final Plans)
37	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(8)(R)	Identification of any lands to be dedicated
38	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(c)(4) 505.10(9)(D) (iv) 507.7(1)(c)	Provide a Landscaping Plan

The following when located ***within fifty feet of the site:***

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
39	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(3)(A) 505.10(6)(A)	Approximate location and name of existing rights-of-way and cartways for streets, access drives and service streets
40	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(3)(B) 505.10(6)(B)	Sanitary Sewer mains
41	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(3)(B) 505.10(6)(B)	Water supply mains
42	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(3)(B) 505.10(6)(B)	Fire hydrants
43	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(3)(D) 505.10(6)(C)	Storm water management facilities which effect storm water runoff on the site. Include the size, capacity and condition of existing storm water management system

The following when located ***within the site:***

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
44	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(4)(A) 505.10(7)(A)	The location, name, and dimensions of existing rights-of-way and cartways for streets, access drives, driveways, and service streets
45	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(4)(A) 505.10(7)(B)	Sanitary sewer mains
46	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(4)(A) 505.10(7)(B)	Water supply mains
47	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(4)(A) 505.10(7)(B)	Fire hydrants
48	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(4)(A) 505.10(7)(B)	Buildings
49	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(4)(D) 505.10(7)(B)	Storm water management facilities
50	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(4)(C) 505.10(7)(C)	The location of existing rights-of-way for electric, gas, and oil transmission lines and railroads
51	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.5(4)(D) 505.10(7)(D)	The size, capacity and condition of the existing storm water management system

Certificates and Notifications

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
52	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(b) 505.10(9)(B)	Correspondence from the owner or lessee of any electric transmission lines, gas pipelines, or petroleum projects transmission lines, located within the tract, stating any conditions on the use of the land and the minimum building setback and/or right-of-way line
53	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(d) 505.10(9)(E)	Traffic Impact Study, if required by City Engineer
54	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(9)(A)	Notification of approval from PA D.E.P. for any storm water management facilities that effect an existing watercourse or have an upland drainage area greater than one-half mile; or that no approval is required
55	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(9)(J)	For plans which require access to a State Highway, the inclusion of the following plan note: "A Highway Occupancy Permit is required pursuant to Section 420 of the Act of June 1, 1945 (P.L. 1242, No. 428), known as the "State Highway Law", before driveway access to a State Highway is permitted. Access to the State highway shall only be as authorized by a Highway Occupancy Permit, and City Council's/Planning Commission's approvals of this plan implies that such permit can be acquired."

Natural Features Preservation

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
56	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.7(1)(A)	Trees with a caliper of six inches (6") or more, measures at a height of 4 1/2 feet above grade, are to be retained, if not within proposed cartway, or sidewalk portion of street right-of-way, or within 15 feet of a proposed building
57	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.7(1)(B)	Where existing trees are removed along the street right-of-way, supplemental plantings required
58	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.7(2)(A)	Maintenance easement provided along all stream, river banks and lake edges. Minimum width of easement to be 25 feet
59	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.7(2)(B)	Water frontage to be preserved as open space whenever possible
60	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.7(2)(C)	Access to be provided to the water and to maintenance easement area. Width of access points to be minimum of fifty feet
61	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.7(3)	Existing terrain to be preserved when possible. Cut and fill to be kept to a minimum

ADDITIONAL SUBDIVISION AND LAND DEVELOPMENT REQUIREMENTS

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
62	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(f)	For Preliminary Plan calling for installation of improvements beyond two years, a schedule of the timing of the improvements
63	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(9)(A)	Notification from PA D.E.P. that either approval of the Sewer Facility Plan Revision or Supplement has been granted or that such approval is not required (Plan Revision Modular for Land Development)
64	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(8)(D)	For Land Development Plans, the location and configuration of proposed buildings, parking compounds, streets, access drives, driveways, and all other significant planned facilities
65	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(8)(Q)	For Land Development Plans, a grading plan, which shall

					include finished grades and ground floor elevations (may be provided on separate sheets, need not be recorded)
66	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(9)(H)	Approval from the City Solicitor of the Improvement Construction Guarantee

Land Development Plans **must include** drawings of the proposed development showing:

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
67	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(c)(4) 505.10(9)(D)(iv) 507.7(1)(c)	Landscape Plan showing treatment of private and common open space, the perimeter of the site, and elevation drawings highlighting the landscaping
68	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(c)(1) 505.10(9)(D)(i)	Elevation of all visible sides, including facade materials
69	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(c)(2) 505.10(9)(D)(ii)	Location and floor area of all existing and proposed buildings, structures, and other improvements, including height, types of units and uses
70	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(c)(3) 505.10(9)(D)(iii)	Details of significant architectural features
71	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(c)(5) 505.10(9)(D)(v)	Vehicular and Pedestrian Circulation Plan, including parking, service areas, waiting areas, major access points
72	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.7(c)(6) 505.10(9)(D)(vi)	Lighting, screening, drainage and dimensions of all off-street parking areas

Design of the Utility Plan, which shall include the size, material, and vertical and horizontal locations of sanitary sewer, storm sewer and water mains:

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
73	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(8)(K)(i)	Include all calculations, assumptions, criteria and references used in the design of stormwater management facilities, the establishment of existing facilities capacities, and the pre and post development discharge
74	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(8)(K)(ii)	All plans and profiles of the proposed storm water management facilities
75	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(8)(K)(iii)	For all basins, a plotting or tabulation of the storage volumes and discharge curves with corresponding water surface elevations, inflow hydrographs, and outflow hydrographs
76	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(8)(K)(iv)	Guidelines for lot grading within the subdivision, identifying the direction of stormwater runoff within each lot, and the areas where runoff will be concentrated (show using topographic data)
77	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(8)(K)(v)	Soil characteristics report and information on erosion and sediment control from the Dauphin County Soil and Water Conservation District

Blocks

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
78	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.4(b)	Blocks shall not exceed 750 feet and must be compatible with the City's existing grid system
79	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.4(c)	Residential blocks should be of sufficient depth to accommodate 2 tiers of lots

Lots

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
80	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.5(a)(1)	Size, depth, width and orientation of lots shall conform to City Zoning requirements
81	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.5(a)(2)	Side lot lines should be at right angles to straight street lines or

					radial to curved street lines, if practical
82	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.5(a)(3)	Where feasible lot lines should follow municipal boundaries rather than cross them
83	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.5(a)(4)	Remnants of land shall be incorporated into existing or proposed lots
84	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.5(b)(1)	All lots shall abut a street, existing or proposed
85	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.5(b)(2)	Double or reverse frontage lots shall be avoided
86	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.5(d)	Nonresidential lots shall provide adequate space for yards, off-street loading and unloading and for parking

Building Design Guidelines

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
87	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.6(1)	Compatibility with existing structures
88	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.6(2)	Design incorporates architectural details found on adjacent buildings
89	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.6(3)	Building materials reflect surrounding development

STREETS, ACCESS STREETS, AND DRIVES ADDITIONAL REQUIREMENTS

	Provide d	No / Waiver Requested	N/A	Ordinance	Standards															
90	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(9) 505.10(l)	Typical cross section of each street proposed or to be improved as part of the application. Cross section shall include entire right-of-way width															
91	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(10)	Street centerline profile for each proposed street shown on the plan															
92	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(1) 505.7(a)	Sketch of future street system, where Preliminary Plan covers only a part of the entire landholdings															
93	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(9)(C)	Notice from postmaster that the proposed street names are acceptable															
94	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(9)(l)	Maintenance agreement for street which is not to be offered for dedication															
95	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(a)(5)	Street arrangement provides for continuation of streets in adjoining areas															
96	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(a)(5)	Provide for proper projection of streets where adjoining areas are not subdivided															
97	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(a)(5)	New streets carried to the boundaries of the tract to be subdivided															
98	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(a)(7)	Residential alleys are discouraged, unless site design calls for rear access parking. 22 foot minimum width if used															
99	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(a)(7)	Adequate alleys or off-street loading and unloading required in commercial or industrial areas															
100	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2 (c)(1)	Cartways to be minimum of 11 foot travel lanes															
101	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(c)(3)	Minimum street rights-of-way and cartway widths shall be: <table><tr><td><u>STREET TYPE</u></td><td><u>R-O-W WIDTH</u></td><td><u>CARTWAY</u></td></tr><tr><td>Arterial</td><td colspan="2">determined by governing body after consultation with PennDOT</td></tr><tr><td>Collector</td><td>60 feet</td><td>22 feet</td></tr><tr><td>Local</td><td>50 feet</td><td>22 feet</td></tr><tr><td>Alley/service drive</td><td>30 feet</td><td>22 feet</td></tr></table>	<u>STREET TYPE</u>	<u>R-O-W WIDTH</u>	<u>CARTWAY</u>	Arterial	determined by governing body after consultation with PennDOT		Collector	60 feet	22 feet	Local	50 feet	22 feet	Alley/service drive	30 feet	22 feet
<u>STREET TYPE</u>	<u>R-O-W WIDTH</u>	<u>CARTWAY</u>																		
Arterial	determined by governing body after consultation with PennDOT																			
Collector	60 feet	22 feet																		
Local	50 feet	22 feet																		
Alley/service drive	30 feet	22 feet																		
102	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(d)(3) 507.2(d)(5)	Permanent dead-end streets not longer than 500 feet and provided with a cul-de-sac, having minimum outside road diameter of 80 feet															
103	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(e)(3)	Tangents between curves according to Engineering															

					specifications
104	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(f)(1)	Centerlines of streets intersect as nearly to 90 degrees as possible, but not less than 75 degrees, or more than 105 degrees
105	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(f)(2)	Intersections involving the junction of more than two streets are prohibited
106	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(f)(3)	Street jogs with centerline off-sets of less than 125 feet avoided
107	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(f)(5)	Intersections with major streets shall be located not less than 800 feet apart
108	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(h)	Street grades in accordance with Engineering specifications
109	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.6(14)	New street names shall not duplicate existing names
110	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(j)(1)	Direct access from residential lots to arterials shall be avoided
111	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(j)(2)	Driveways on corner lots shall be located 40 feet from the point of intersection of the nearest street right-of-way
112	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(j)(3)(A)	Access roads/driveways shall be: No less than 24 feet in width for multi-family residential, and all non-residential development
113	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(j)(3)(B)	Access roads/driveways shall be no less than 10 feet for single family residential subdivisions, and no greater than 20 feet at the street line
114	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.2(j)(5)(A&B)	Grade on access road/driveway shall not exceed 8% on arterial and 10% on collector or minor street

EASEMENT REQUIREMENTS

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
115	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.3(a)(1)	Minimum width shall be twenty feet (20 ft)
116	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.3(a)(2)	When possible, easements shall be centered on the side or rear lot lines or along the front lot lines
117	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	507.3(b)	If subdivision is traversed by a water course or drainageway, an easement shall be provided conforming with the line of such water course or drainageway

CONDOMINIUM PROJECT REQUIREMENTS

	Provided	No / Waiver Requested	N/A	Ordinance	Standards
118	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.8(4)	Floor plans for all buildings
119	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.8(3)	Statement of proposed use for each unit and restrictions on use, if any
120	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.8(2)	Description of common elements, with allocation of the proportionate undivided interest expressed as a percentage assigned to each unit
121	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.8(1)	Statement of intent to submit the property to the provisions of the Unit Property Act, Act 117 of 1963, as amended (Preliminary Plan)
122	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	505.10(10)	Declaration, Declaration Plan and Code of Regulations required by the Unit Property Act, Act 117 of 1963 as amended (Final Plan - may be provided on a separate sheet)